

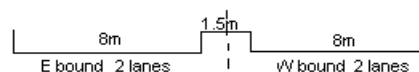
YEAR 2004

COVERAGE (B) STATION 4221

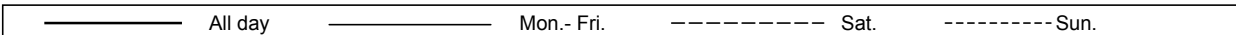
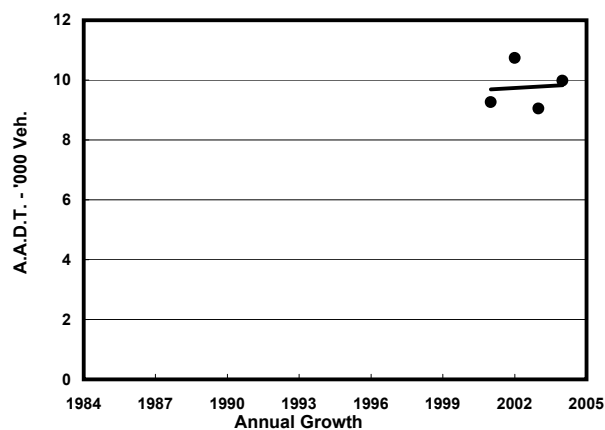
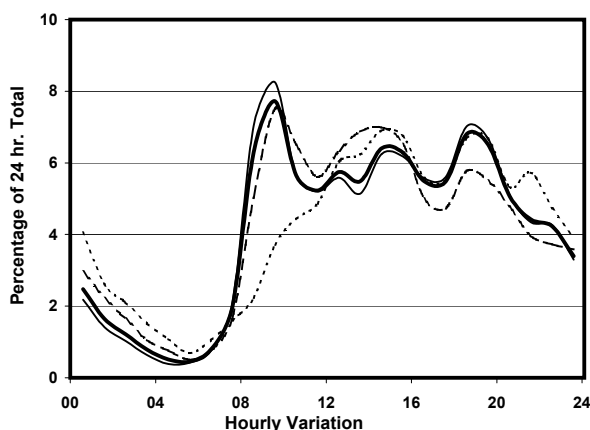
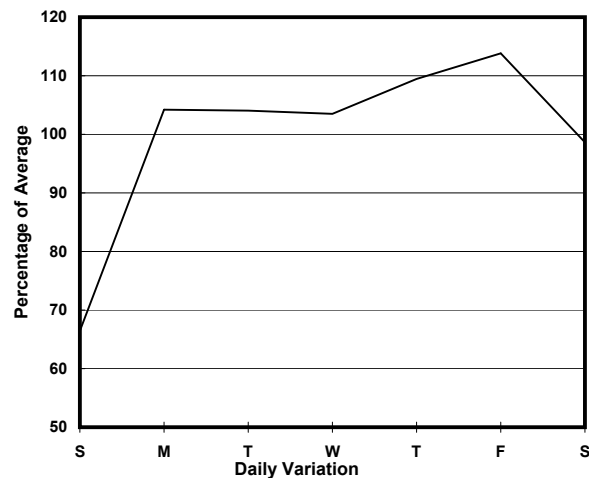
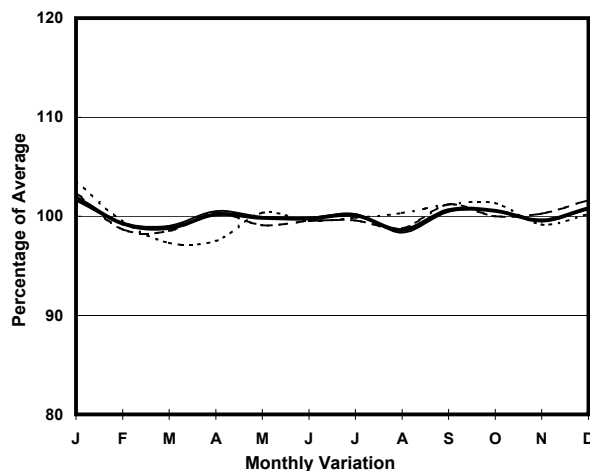
ROAD NETWORK MAJOR

ROAD TYPE EXPRESSWAY

LINK HUNG HOM BYPASS (from SLIP RD TO CHEONG TUNG RD S to SLIP RD FROM CHEONG TUNG RD S)



1. TRAFFIC FLOW VARIATION AND GROWTH



2. TRAFFIC CHARACTERISTICS (BY DIRECTION)

Parameter	All - Day	Mon. - Fri.	Sat.	Sun.
EAST BOUND				
A.A.D.T.	4810	5130	4980	3620
R 12 / 24 - %	66.2	66.9	66.8	60.5
R 16 / 24 - %	88.6	89.8	86.3	84
AM Peak Hour	0900-1000	0900-1000	0900-1000	0900-1000
One-way flow at AM peak hour	370	430	350	140
T - % (AM)	-	9.1	-	-
PM Peak Hour	1800-1900	1800-1900	1800-1900	1800-1900
One-way flow at PM peak hour	320	350	280	250
T - % (PM)	-	4.2	-	-
Prop.of commercial vehicles - 16 hr.	-	6.6	-	-
WEST BOUND				
A.A.D.T.	5170	5730	5030	3140
R 12 / 24 - %	70.6	71.8	70.2	60.7
R 16 / 24 - %	88.8	90	86.7	81.9
AM Peak Hour	0900-1000	0900-1000	0900-1000	0900-1000
One-way flow at AM peak hour	400	470	410	110
T - % (AM)	-	8.4	-	-
PM Peak Hour	1800-1900	1800-1900	1800-1900	1800-1900
One-way flow at PM peak hour	360	410	300	200
T - % (PM)	-	5.1	-	-
Prop.of commercial vehicles - 16 hr.	-	5.4	-	-

3. OTHER INFORMATION AND COMMENT

Coverage (B) Station 4221
Year 2004

4. Vehicle classification and occupancy - Monday to Friday

Time		Class of vehicle									
		Motor	Private	Taxi	Private	PLB	Goods veh.		Non	Fr. Bus	
		Cycle	Car		LB		Light	M & H	Fr. Bus	SD	DD
0700-0800	Pro	0.0	12.1	58.9	0.0	1.5	10.6	3.0	9.1	0.4	4.5
	Ocp	0.0	1.3	1.8	0.0	1.0	1.6	2.0	1.2	15.0	13.5
0800-0900	Pro	1.5	12.9	74.8	0.0	0.0	5.9	0.5	2.0	0.4	2.0
	Ocp	1.3	1.7	1.5	0.0	0.0	1.8	1.0	1.3	15.0	27.7
0900-1000 Peak hour	Pro	0.6	14.8	59.3	1.2	0.0	15.4	4.7	1.8	0.4	1.8
	Ocp	1.0	1.6	1.9	1.0	0.0	1.8	1.5	5.0	17.0	13.3
1000-1100	Pro	4.8	20.4	46.2	2.7	0.7	21.7	1.4	0.7	0.5	1.0
	Ocp	1.0	1.5	1.8	2.5	1.0	1.8	1.5	1.0	11.0	10.3
1100-1200	Pro	1.3	21.3	46.4	3.2	0.0	23.2	1.3	1.9	0.3	1.1
	Ocp	1.0	1.5	1.8	1.6	0.0	1.7	1.5	8.0	10.0	8.3
1200-1300	Pro	0.7	25.7	45.3	0.0	0.7	17.6	3.4	5.4	0.0	1.4
	Ocp	1.0	1.5	1.8	0.0	1.0	1.8	1.4	7.8	0.0	8.6
1300-1400	Pro	2.5	24.3	41.9	0.0	0.8	21.8	5.0	1.7	0.0	1.9
	Ocp	1.0	1.8	1.5	0.0	1.0	1.7	1.7	14.5	0.0	7.0
1400-1500	Pro	3.6	27.1	43.3	0.0	0.6	19.9	1.8	2.4	0.0	1.4
	Ocp	1.2	1.8	1.7	0.0	2.0	1.8	2.0	5.5	0.0	8.6
1500-1600	Pro	1.9	31.0	40.4	0.6	0.0	20.9	1.9	1.9	0.0	1.4
	Ocp	1.0	1.7	1.8	1.0	0.0	1.3	2.0	2.0	0.0	8.7
1600-1700	Pro	2.3	30.4	39.7	0.0	3.1	17.1	1.6	3.9	0.4	1.6
	Ocp	1.0	1.3	1.8	0.0	4.0	1.9	1.5	2.8	20.0	9.1
1700-1800	Pro	1.3	29.0	45.8	1.3	2.0	13.5	0.7	4.7	0.5	1.2
	Ocp	1.0	1.4	1.9	1.0	1.0	1.9	2.0	5.1	9.3	12.6
1800-1900	Pro	3.2	26.8	57.8	0.5	2.7	4.3	0.0	3.2	0.3	1.2
	Ocp	1.0	1.5	2.1	1.0	4.6	1.1	0.0	13.7	28.0	39.2
1900-2000	Pro	1.8	28.4	59.8	0.0	3.0	4.7	0.6	0.0	0.3	1.5
	Ocp	1.7	1.7	2.3	0.0	6.8	1.3	1.0	0.0	8.5	27.2
2000-2100	Pro	1.7	30.6	59.4	1.7	0.0	2.6	0.0	1.7	0.7	1.5
	Ocp	1.0	1.7	1.9	1.0	0.0	2.0	0.0	1.0	16.3	20.7
2100-2200	Pro	0.0	22.8	68.3	0.0	1.0	3.0	0.0	3.0	0.2	1.7
	Ocp	0.0	1.7	2.3	0.0	5.0	1.3	0.0	1.0	36.0	10.9
2200-2300	Pro	1.9	30.9	62.8	0.0	1.0	0.0	0.0	1.0	0.0	2.4
	Ocp	1.5	1.6	1.6	0.0	2.0	0.0	0.0	14.0	0.0	6.5
16 hours	Pro	1.9	24.2	53.1	0.7	1.1	12.9	1.6	2.5	0.3	1.6
	Ocp	1.1	1.6	1.9	1.5	3.7	1.7	1.6	5.6	15.6	15.4

Legend

Pro. Proportion of vehicles in % (Sum may not add up to 100% due to figure rounding)
Ocp. Average occupancy of vehicles
M&H Medium and Heavy