

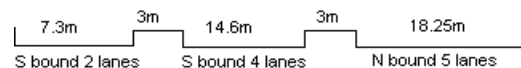
YEAR 2004

CORE STATION 5026

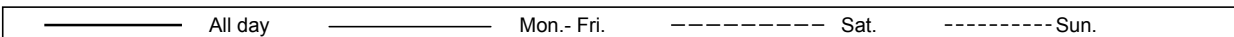
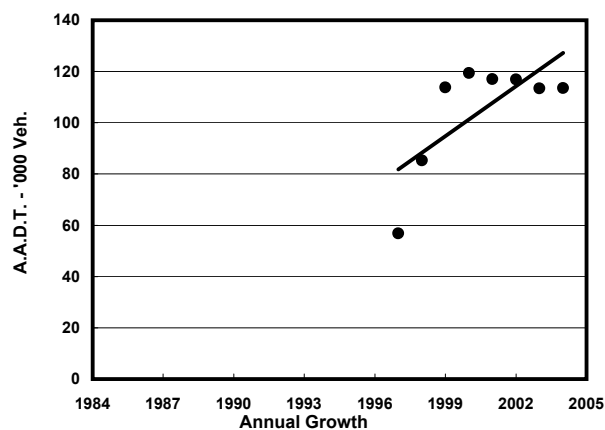
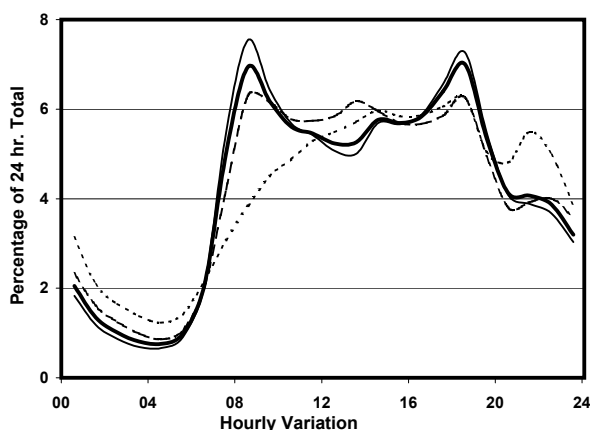
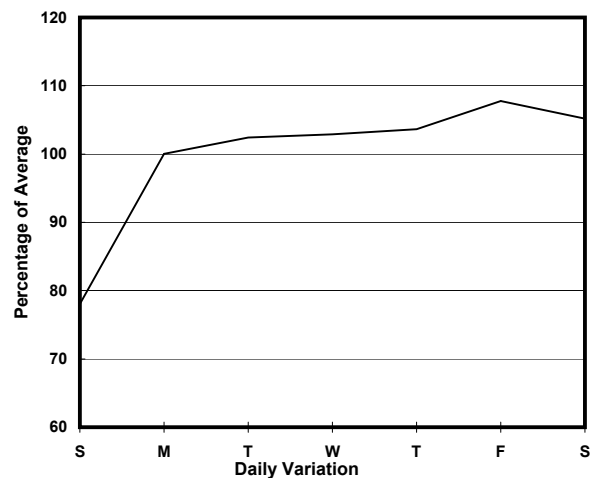
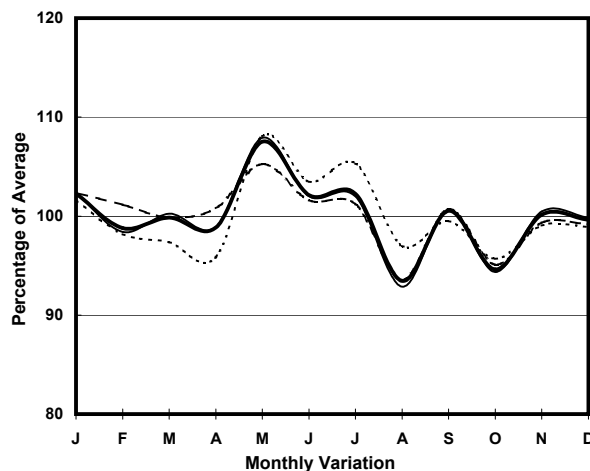
ROAD NETWORK MAJOR

ROAD TYPE EXPRESSWAY

LINK TSING KWAI HIGHWAY (from CHING LAI COURT SLIP
RDS TO & FROM CHING CHEUNG RD to CHO YIU
CHUEN SLIP RDS TO & FROM KWAI CHUNG RD &
TSUEN WAN RD)



1. TRAFFIC FLOW VARIATION AND GROWTH



2. TRAFFIC CHARACTERISTICS (BY DIRECTION)

Parameter	All - Day	Mon. - Fri.	Sat.	Sun.
SOUTH BOUND				
A.A.D.T.	61060	64130	65240	46590
R 12 / 24 - %	74.3	75.6	73.2	66.8
R 16 / 24 - %	89.4	90.3	88.4	85.2
AM Peak Hour	0800-0900	0800-0900	0800-0900	0900-1000
One-way flow at AM peak hour	4940	5620	4900	2310
T - % (AM)	-	24.6	-	-
PM Peak Hour	1800-1900	1800-1900	1800-1900	1800-1900
One-way flow at PM peak hour	4230	4540	4140	3100
T - % (PM)	-	21.2	-	-
Prop.of commercial vehicles - 16 hr.	-	29.3	-	-
NORTH BOUND				
A.A.D.T.	52470	54610	55540	43100
R 12 / 24 - %	65.9	67.1	65.5	58.5
R 16 / 24 - %	85.9	87	83.9	80.9
AM Peak Hour	0800-0900	0800-0900	0900-1000	0900-1000
One-way flow at AM peak hour	2940	3340	2910	1740
T - % (AM)	-	25.2	-	-
PM Peak Hour	1800-1900	1800-1900	1800-1900	1700-1800
One-way flow at PM peak hour	3730	4090	3420	2540
T - % (PM)	-	26.4	-	-
Prop.of commercial vehicles - 16 hr.	-	27	-	-

3. OTHER INFORMATION AND COMMENT

4. Vehicle classification and occupancy - Monday to Friday

Time		Class of vehicle									
		Motor	Private	Taxi	Private	PLB	Goods veh.		Non	Fr. Bus	
		Cycle	Car		LB		Light	M & H	Fr. Bus	SD	DD
0700-0800	Pro	3.3	23.3	17.5	1.9	0.1	16.9	23.9	8.3	0.0	4.8
	Ocp	1.1	1.2	2.1	5.6	14.0	1.2	1.1	21.8	0.0	48.1
0800-0900 Peak hour	Pro	2.7	50.9	9.5	0.8	0.0	11.2	15.2	5.7	0.1	4.0
	Ocp	1.2	1.2	2.1	7.9	0.0	1.5	1.1	21.5	15.0	51.6
0900-1000	Pro	1.9	38.0	11.5	1.0	0.0	19.9	18.4	4.7	0.0	4.6
	Ocp	1.1	1.3	2.0	3.2	0.0	1.4	1.2	17.6	0.0	39.9
1000-1100	Pro	0.5	37.0	7.6	0.6	0.0	24.2	24.3	2.2	0.0	3.8
	Ocp	1.0	1.4	2.0	5.3	0.0	1.5	1.2	9.0	0.0	24.3
1100-1200	Pro	1.1	33.1	9.3	1.4	0.0	24.2	23.7	3.0	0.0	4.0
	Ocp	1.1	1.3	2.0	5.1	0.0	1.4	1.2	12.2	0.0	21.2
1200-1300	Pro	1.4	34.7	12.0	1.0	0.0	20.5	22.8	4.0	0.0	3.6
	Ocp	1.2	1.5	2.0	3.0	0.0	1.5	1.1	20.6	0.0	20.9
1300-1400	Pro	1.8	33.0	10.7	0.7	0.0	23.3	22.2	4.2	0.0	4.1
	Ocp	1.1	1.4	2.2	4.9	0.0	1.3	1.2	16.8	0.0	28.4
1400-1500	Pro	1.8	32.4	10.4	1.3	0.0	21.6	26.0	3.2	0.0	3.3
	Ocp	1.1	1.5	1.8	2.1	0.0	1.3	1.1	13.4	0.0	29.0
1500-1600	Pro	1.3	34.6	10.4	1.1	0.0	21.5	24.8	2.6	0.0	3.5
	Ocp	1.0	1.3	1.9	2.4	0.0	1.4	1.1	19.4	0.0	21.5
1600-1700	Pro	1.6	35.4	11.1	1.2	0.0	21.6	20.4	4.8	0.0	3.9
	Ocp	1.0	1.3	1.8	2.4	0.0	1.4	1.2	10.4	0.0	28.6
1700-1800	Pro	2.3	39.3	10.1	0.9	0.0	17.8	20.5	5.0	0.0	4.2
	Ocp	1.1	1.3	1.7	2.4	0.0	1.2	1.1	12.1	0.0	35.1
1800-1900	Pro	2.0	50.0	8.3	0.9	0.1	15.2	13.7	6.1	0.1	3.7
	Ocp	1.2	1.3	2.0	2.3	17.0	1.2	1.1	29.4	9.5	47.9
1900-2000	Pro	2.0	50.4	8.0	0.1	0.1	13.3	15.3	6.0	0.1	4.8
	Ocp	1.2	1.2	1.8	1.0	17.0	1.3	1.1	28.6	13.5	35.2
2000-2100	Pro	2.4	45.4	10.7	0.3	0.1	11.1	19.0	4.8	0.1	6.2
	Ocp	1.1	1.3	1.8	1.7	10.0	1.3	1.1	14.5	21.0	23.9
2100-2200	Pro	1.5	52.4	13.3	0.3	0.0	11.4	11.4	4.7	0.1	5.1
	Ocp	1.1	1.1	1.8	3.3	0.0	1.2	1.1	11.9	16.0	27.4
2200-2300	Pro	1.5	58.0	15.2	0.1	0.0	9.2	7.3	3.0	0.1	5.7
	Ocp	1.1	1.2	1.7	12.0	0.0	1.2	1.1	14.8	16.0	24.7
16 hours	Pro	1.8	40.5	10.6	0.9	0.1	17.9	19.5	4.5	0.1	4.2
	Ocp	1.1	1.3	1.9	3.8	15.0	1.4	1.1	18.7	14.3	32.5

Legend

- Pro.** Proportion of vehicles in % (Sum may not add up to 100% due to figure rounding)
- Ocp.** Average occupancy of vehicles
- M&H** Medium and Heavy